

The logging branch is non-electrified territory. Either a logging run from Avery can service the branch, or a dedicated switcher can feed cars to mainline trains. The O-27 curves are large enough for only small motive power and rolling stock

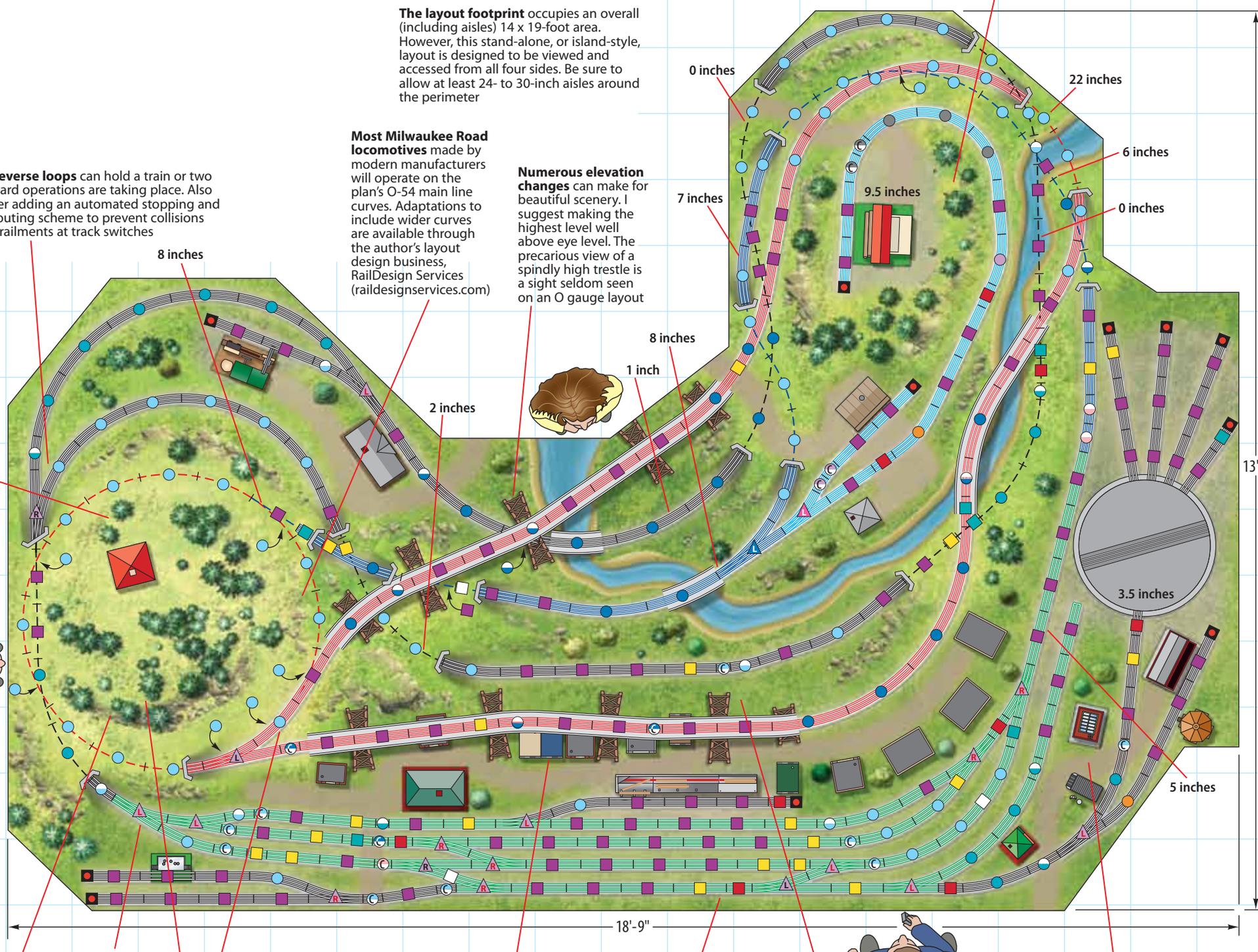
The layout footprint occupies an overall (including aisles) 14 x 19-foot area. However, this stand-alone, or island-style, layout is designed to be viewed and accessed from all four sides. Be sure to allow at least 24- to 30-inch aisles around the perimeter

Most Milwaukee Road locomotives made by modern manufacturers will operate on the plan's O-54 main line curves. Adaptations to include wider curves are available through the author's layout design business, RailDesign Services (raildesignservices.com)

Numerous elevation changes can make for beautiful scenery. I suggest making the highest level well above eye level. The precarious view of a spindly high trestle is a sight seldom seen on an O gauge layout

Long reverse loops can hold a train or two while yard operations are taking place. Also consider adding an automated stopping and track-routing scheme to prevent collisions and derailments at track switches

In a helix, each turn raises the track level about 6 inches. A rise of 12 inches above the lower electric line yields enough elevation to make impressive high trestles



Mountain scenery is a delight to view, but it can quickly become tedious work. Fortunately, several scenery manufacturers that offer great-looking conifers in a variety of types and sizes

Need access to the reverse loop switch? In the rare case of derailment, use a grabber to retrieve cars along the high line

A mountain hamlet nestled below a high trestle is a space saver that also provides great visual impact

The yard at Avery can serve as a site for switching, swapping locomotives, or storing (staging) trains while they wait for their chance to run on the main line

Using stacked loops rather than hidden tunnel trackwork helps keep the action visible. Although some of the trackwork can be hidden for "backstage" train storage, most operators don't mind having trains always in view

Servicing facilities for steam, diesel, and electric locomotives are present at Avery

Track intended for steam, electric, or both means of motive power is color-coded on the plan. Trackwork in black indicates a steam-only route, while blue and red tracks indicate routes used by electric locomotives. Routes marked in green can be used by either type of motive power.